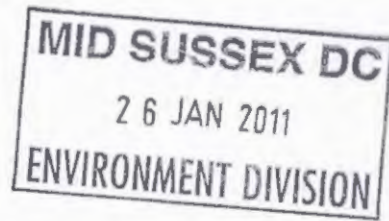


04/02964/FUL

DCCONDP

09/02101/COND.

STMA



**CONSTRUCTION  
MANAGEMENT  
PLAN**

**Bolnore Village,  
Phase 4**

Prepared By

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On Behalf Of

**CREST  
NICHOLSON  
SOUTH**

January 2011

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## 1.0 Introduction

- 1.1 Condition 32 of application **09/02101/COND** stipulates that a Construction Management Plan (CM) is required in order to safeguard the amenities of nearby residents in relation to Policy B3 of Mid Sussex Local Plan.
- 1.2 This document has therefore been prepared in order to discharge the aforementioned condition as part of the Phase 4A Reserved Matters application process with this document referring to the development of all elements of Phase 4.
- 1.3 The production of this document has been based upon work relating to construction management which forms a key chapter within the associated Environmental Impact Assessment (June 2009).
- 1.4 Based on previous experience of construction traffic associated with other large sites, the CMP provides greater detail about the actual type and level of construction traffic that will be generated as the site is developed and the favoured route into and out of the site is established.
- 1.5 This document is therefore designed to provide clarification for West Sussex County Council (WSCC), Mid Sussex District Council (MSDC), local amenity groups, local residents and all those involved in the development process in relation to construction traffic and how disruption will be minimised.



## 2.0 Proposed Development

- 2.1 The proposed scheme for Phase 4 is based upon the development of a maximum of 543 residential units as outlined by Condition 44 of Application **09/02101/COND**.
- 2.2 The Spine Road relates to Application **HH/04/02964** which was a full application for infrastructure on Phase 4.
- 2.3 Both applications **09/02101/COND** and **HH/04/02964** have now been approved with decision notices attached as **Appendix 1**.
- 2.4 Whilst this ceiling level of units is in place it is considered that the eventual development, once planning permission has been granted would consist of the following indicative unit numbers:

**Table 1: Phases and Unit Numbers**

Phase	Indicative No. of Units
<b>4A</b>	197
<b>4B</b>	121
<b>4C</b>	80
<b>Total</b>	398

- 2.5 The next section of this statement will outline the key construction stages as well as the routes to be used for construction traffic.

### 3.0 Construction Traffic Routing

- 3.1 The development of the site will be undertaken in accordance with Conditions 33 and 34 of Application **09/02101/COND**. These conditions outline that construction access arrangements will be in two stages as outlined below:

#### Stage 1

**Development:** Construction of only the Spine Road linking Bolnore Road to Parkfield Way (completed to base course level).

**Route:** Two-way working – Vehicles in relation to the construction of the Spine Road in Phase 4B to enter and exit the site only via Bolnore Road (**Appendix 2**).

Vehicles in relation to the construction of the Spine Road in Phase 4A to enter and exit the site only via Parkfield Way (**Appendix 2**).

#### Stage 2

**Development:** All other construction relating to infrastructure and residential units on Phase 4.

**Route:** One-way working – All vehicles to enter the site via Bolnore Road and exit via Parkfield Way (**Appendix 3**).

- 3.2 Bolnore Road will also be used to provide construction access for the aged persons units to allow them to be built independently and at an early stage.
- 3.3 To facilitate the access arrangements on Parkfield Way a number of temporary works will be undertaken to ensure that larger vehicles can enter and leave the site safely. These changes are shown in **Appendix 4** along with a tracking route for an articulated lorry **Appendix 5**.

#### 4.0 Indicative Vehicle Type and Numbers

- 4.1 This section will outline the indicative number and type of vehicles that will be involved during the development stages as previously outlined. Examples of the types of vehicle can be found in **Appendix 6**.

##### Stage 1 – Spine Road

- 4.2 The first element of this construction phase would be to construct the Bell Mouth on Bolnore Road where the entry to the site is located. This would involve two-way access along Bolnore Road using the vehicles outlined in **Table 2**.

**Table 2 Bell Mouth Construction**

Type of Vehicle	Total number of deliveries over the 6 week period of construction
360° Turner	1
Dump Trucks	1
JCB	1
Deliveries of hardcore using rigid wheelbase	50
Deliveries of tarmac using rigid wheelbase HGV	20
Gang of men with roller	10
Daily visits from individual contractors	20

- 4.3 The second element of this stage would be the construction of the Internal Spine road over a period of six months. This would involve two-way access to and from Phase B from Bolnore Road and two-way access to and from Phase 4A from Parkfield Way.
- 4.4 The vehicle movements that will be associated with this element of the scheme are outlined in **Table 3**:

**Table 3 Spine Road Construction to Base Course**

Type of Vehicle	Total number of deliveries over the 6 month period of construction
360° Turner	4
Dump Trucks	4
JCB	1
Deliveries of hardcore using rigid wheelbase	300
Deliveries of tarmac using rigid wheelbase HGV	120
Gang of men with roller	10
Daily visits from individual contractors	20



## Stage 2 – Housing Phases

- 4.5 The 197 units on Phase 4A would then be built out, followed by those on the remaining Phases subject to Reserved Matters approval. Prior to this stage of the development the Spine Road will have been completed to base course level and therefore access will be via the one-way working mechanism.
- 4.6 The indicative vehicles to be used for the remainder of the development are likely to be as follows:

### Land Grading

- 4.7 Crest Nicholson operates to best practice guidance and therefore removes the best areas of topsoil from development sites before commencing construction. This topsoil is usually stockpiled on site and used for redressing the landscape areas after construction has been completed. The levels associated with the Phase 4A site mean that in addition to the scraping off of topsoil, land re-grading works will also be required.
- 4.8 It is anticipated that the required land re-grading stage for the development of Phase 4A will last approximately three months in total with a similar time period expected upon Phase 4B. This activity will continue once the initial land grading has taken place to enable foundations and construction to commence. Vehicles associated with this work are outlined in **Table 4**.

**Table 4 Land Re-Grading**

Type of Vehicle	Total number of deliveries over the 3 month period of construction
360° Turner	4
Dump Truck	1
8 Wheel Lorries for muck removal	Up to 80 per day
Daily visits from individual contractors	15 – 20 per day

- 4.9 The 360° Turner and dump truck would have already been delivered to site by low loader at the time of the construction of the access point into the site and so will not generate additional traffic movements to and from the development.
- 4.10 The only traffic movements to and from the site associated with land re-grading will therefore be those associated with contractors entering and leaving the site on a daily basis, and lorries used for muck removal.

### Units and Infrastructure

- 4.11 All vehicles involved with the construction of the remaining units would use the one-way working system. The type and number of vehicles are outlined in **Table 5**.

**Table 5      Unit and Infrastructure Development**

Type of Vehicle	Number of vehicles
Rigid lorries supplying bricks, foundation beams and blocks	10 per week
Articulated lorries supplying joists and prepared timber products	2 per week
axle trucks for re-grading and any muck removal	25 per week
Concrete mixers for batch foundations e.g. 4 units	25 per day
Hi-ab deliveries for other miscellaneous materials	2 per week
Low loader for the delivery of a piling rig	1 (once on site this will remain for the duration of the foundation works)
Heavy goods vehicles for scaffolding	5 per week
Skip lorries	10 per week
Deliveries of hardcore using rigid wheelbase HGV	to 4 per day (only towards the end of the build programme)
Deliveries of tarmac using rigid wheelbase HGV	2 to 3 per day (only towards the end of the build programme)
Kitchen delivery and fitting	three vehicles per week
Bathroom delivery and fitting	three vehicles per week
Carpet/flooring delivery and fitting	three vehicles per week



## **5.0 Construction and Traffic Controls**

- 5.1 In order to ensure that construction traffic is carefully managed the following controls will be put in place to ensure high levels of safety and minimal disturbance for residents.
- 5.2 These controls are based on the Statement of Common Ground dated 27<sup>th</sup> April 2007 and agreed between Crest Nicholson and Local Community Organisations which are further supported by the respective conditions attached to application **09/02101/COND** as subsequently referred to.

### **Working Hours**

- 5.3 No work for the implementation of the development hereby permitted shall be undertaken on the site on Public Holidays.
- 5.4 Work shall not be undertaken at any other time except between the hours of 08.00 and 18.00 on Mondays to Fridays and between 08.00 and 13.00 on Saturdays, unless otherwise agreed in writing by the local planning authority (Condition 28).

### **Site Access**

- 5.5 Access to the site for HGV's over 7.5 Tonnes will be limited to between 09:15am and 15:30pm, Mondays to Fridays; (Condition 29).
- 5.6 Access to the site for heavy plant over 7.5 Tonnes will be limited to between 11:00am and 15:00pm, Mondays to Fridays; (Condition 29).
- 5.7 Access to the site for articulated vehicles (other than heavy plant) will be limited to between 09:15am and 15:30pm, Mondays to Fridays (Condition 29).
- 5.8 On occasions a HGV may be required to remove a piece of plant from the site when it is no longer required e.g. a dump truck. Removing such a vehicle on a Saturday is considered to be preferential in respect of safety given the lower volume of traffic at weekends and as the equipment is likely to be in use on site during weekdays.

### **Wheel Washing**

- 5.9 Wheel washing facilities will be located on site at all exit points (Bolnore Road/ Parkfield Way), to ensure that the local road network is kept clear from mud and debris (Condition 30). The location and type of facility used will be agreed with the LPA as part of the condition release process.

### **Temporary Parking and Loading/Unloading Area for Vehicles**

- 5.10 A designated area for the temporary parking and loading/unloading of vehicles (Condition 31) will be provided for contractors parking and agreed as part of the discharge of condition process.

### **Speed Limits**

- 5.11 No construction or construction supply vehicles shall exceed a speed of 10mph within the site, with speed restriction signs being erected by Crest on site.

- 5.12 Advisory 10mph speed restriction signs will be erected on Bolnore Road and Parkfield Way, subject to approval by the Highway Authority.
- 5.13 Crest will take responsibility for enforcing speeds in relation to contractors and supply vehicles within the development and along Bolnore Road and Parkfield Way.

#### **Dust**

- 5.14 To restrict dust creation a number of measures are to be implemented:
- The provision of easily cleaned hard standings for vehicles (to include the areas close to the site access).
  - The regular cleaning by brushing and water spraying of heavily used hard surfaced areas.
  - The provisions of wheel washing facilities to ensure all vehicles leaving the site are in a satisfactory state of cleanliness.
  - Stockpiles of dusty materials and dusty activities such as stone cutting and grinding shall be sited away from the site boundary and/or effectively screened.

#### **Noise**

- 5.15 The Environmental Statement considers the noise effects on nearby residents during the construction period taking account of advice given in BS 5228.
- 5.16 It is concluded that the level of noise from construction traffic from Phase 4 will be audible, but would not cause a detrimental impact to properties on Bolnore Road and Parkfield Way.
- 5.17 Further to the restriction on working hours, as outlined by aforementioned conditions, the following practices can be implemented to restrict possible impacts on local residents:
- 5.18 In order to reduce noise emissions other mitigation measures will be adopted. These will include:
- Selection of the quietest plant where applicable and feasible.
  - All pneumatic percussive tools will be fitted with silencers.
  - All noise emitting equipment that is to run continuously will be housed where feasible in a suitable enclosure.
  - All construction work will be undertaken in accordance with codes of practice for construction work and piling as outlined in BS 5228.
  - All noisy plant will be sited as far as possible from the nearest noise sensitive receivers.



## **Gate Access and Banksman**

- 5.19 The development will require the use of large vehicles to bring plant and materials to the site. As large vehicles will use residential roads to access the site two Banksmen will be used to escort HGV's to and from the site via Parkfield Way during the two way working regime.
- 5.20 It is considered that these Banksmen will be stationed at the junction with Pondside. They will ensure that during two-way working that HGV's are escorted to and from the site and when one-way working is in operation HGV's will be escorted from the site exit to the Barn.
- 5.21 Safety in relation to vehicles accessing the site will also be facilitated by the use of manned gates, where the operative in charge will be in radio contact with the Banksmen to control access to and from the site, to ensure that vehicles safely cross the Bridleway and that pedestrians and cyclists have the right of way.

## **Amendments to the Road Network**

- 5.22 To facilitate the one and two way working strategies the following amendments will be made to ensure safety levels are maintained:
- The footway proposed on Bolnore Road would be constructed at an early stage to segregate pedestrians from heavy construction traffic and other vehicles. This would be supplemented by the provision of a passing bay on the northern side of Bolnore Road, opposite the site frontage.
  - Temporary improvements will be made to the Parkfield Way / Pondside junction to improve access for construction vehicles and reduce potential conflicts with other road users. Other minor improvements will be made to widen the entry onto Bolnore Roundabout, and the entry and exits to the dualled section of Parkfield Way.
  - *As referred in the comments raised by WSCC (21.12.10) the developer will reinstate the original junction layout following the completion of building work on Phases 4 and 5. The mechanism to ensure that these works are completed will form part of the Legal Agreement process with WSCC.*

## **Access Management**

- 5.23 A clear signing strategy will be implemented to direct all construction traffic all the way from the A23 via Ansty Cross right into the site. The signage at the site entrance will include the times when access into the site by construction traffic can be obtained.
- 5.24 In addition to the clearly signposted route for construction traffic, a system of access control for vehicles in excess of 6 wheels will be provided, as was introduced and proved to be effective for the development of Phase 2. To ensure that vehicle sizes are regulated, vehicle lengths and weight will have to be specified as part of the sub-contractor and supplier's orders for Phase 4.
- 5.25 It is acknowledged that the Bridleway is used by local children on route to and from school. Access will be carefully managed to ensure that there is no conflict with local school children, other pedestrians or cyclists using the Ashenground Bridleway.



- 5.26 In addition, the physical works at any crossing points will be designed to achieve a "dead slow" speed at these locations, with any making good being carried out after completion of the development.
- 5.27 The routing information will be passed directly through to all drivers of delivery and contractors' vehicles, including the notification of any third party transport providers.
- 5.28 In the event that after the commencement of the main one-way working restrictions an errant vehicle tries to access the site from Parkfield Way, ignoring signs at the entrance to the site off Bolnore Roundabout, it should be able to turn using the car park on Parkfield Way, opposite 'The Barn'. This would apply to all but the very largest HGVs such as articulated vehicles carrying joists, of which only two a week are expected and therefore unlikely to use the incorrect route.

### **Management of Contractors and Communication**

- 5.29 To ensure that the measures within this CMP and the conditions are understood by the contractors, the CMP will be incorporated into the sub contractor's pre-order meeting.
- 5.30 Also, each supplier will be provided with a copy of the CMP, which will form part of the contract with Crest Nicholson, including any sub-contracting.
- 5.31 The potential to enforce the measures proposed will be at Crest Nicholson's discretion. Crest Nicholson will act diligently at all stages to ensure that contractors are respecting the requirements of the CMP/conditions and will take action as necessary, in proportion to the offence.

### **Housing Painting**

- 5.32 If the 5 'white' houses fronting the construction egress route (known as numbers 1,2,7,8 and 9 Parkfield Way) are discoloured as a result of *construction traffic* during the construction period Crest will pay for the repainting of the external render finish of each of these properties (subject to the prior approval of quotations) and in accordance with the lowest price of three quotes received.
- 5.33 These works if undertaken should be no more frequent than annually for each of the five properties. The combined total of all painting works undertaken over the lifetime of the build should not exceed the overall aggregate cap of £10,000 including VAT.

### **Forum**

- 5.34 A monthly residents forum will be held, chaired for at least the first three meetings by a representative from Crest and subsequently will be run by a technical or site manager.

### **The Considerate Constructors Scheme**

- 5.35 In addition, the way in which the whole site is managed will be underpinned by the principles set down in the Considerate Constructors Scheme, to which Crest Nicholson are fully committed.

- 5.36 This is a national scheme, endorsed by Central Government, and created by the construction industry to improve the industry's image through better, more professional working practices.
- 5.37 Crest Nicholson recognise and support the need for professional working practices, therefore this site will be registered with the Considerate Constructors Scheme (CCS).
- 5.38 This scheme provides for all such registered sites to follow its Code of Practice, the broad principles of which are:
- Considerate – Work to be carried out with consideration to the public, businesses, site personnel and visitors.
  - Environmentally Conscious – Noise to be kept to a minimum with attention to waste management and pollution.
  - Cleanliness – Site to be kept clean and in good order.
  - Good Neighbour – Good communication links with neighbours.
  - Safe – All working practices to be carried out with care and consideration.
  - Responsible – Everyone on site complies with the code.
  - Accountable – Site contact details to be easily accessed.
- 5.39 As a registered member of the scheme, posters will be displayed around the site setting out the Code to which the constructors are committed. The name and telephone number of the Site Manager is clearly displayed on the posters, along with the free phone number of the Scheme Administration Office.
- 5.40 The aim of the scheme is to encourage constructors to undertake their operations and activities in such a way that a higher than satisfactory level of performance is achieved. The scheme provides an additional mechanism for all variety of complaints to be submitted, recorded and passed on to the Site Manager.

#### **Pinch Point**

- 5.41 It is intended that the spine road will be utilised as the route for construction traffic throughout the duration of construction activities on the Phase 4 site.
- 5.42 It has previously been agreed with MSDC that following completion of the spine road, construction traffic will enter the site from Bolnore Road to the north, and exit via Phase 1 / Parkfield Way to the west.
- 5.43 The spine road will therefore need to be constructed as early as possible in the development programme, including the section through the Pinch Point.
- 5.44 It is therefore necessary to construct the Pinch Point proposals as submitted to MSDC in relation to Condition 22 of Appeal B (Submitted July 2009) in their entirety, excluding only the final surfacing course of the carriageway, in order to convey

construction traffic through the Pinch Point, with minimum disruption to the immediate surroundings.

- 5.45 Early approval to the Pinch Point proposals will enable these works to be constructed as an early activity, at the beginning of the development programme.



## 6.0 WSCC Comments

6.1 West Sussex County Council in their letter dated 21.12.10 have commented on the content of the CMP (2010 Version) which was submitted as part of the Phase 4A Reserved Matters application.

6.2 The comments outline that, *"the main details of the CMP including routing of construction traffic is understood to have been previously discussed and agreed. However there are a number of related issues to the CMP and routing that would require clarification prior to the formal discharge of this condition"*. The points raised are now considered with answers provided:

Q1 *It is understood that the temporary accommodation works to the junction of Parkfield Way and Parkfield Way (north) have previously been agreed, as have the works to increase the flare length of the Isaacs Lane approach to the A272 roundabout and reduce the diameter of the roundabout. Works were previously proposed to the A272/B2184 roundabout.*

A1 The Works to the A272/B2184 are still proposed. These, along with the other off-site Highway Works listed above are being re-submitted to WSCC (W/C 24.01.11) for technical approval and commencement of the Legal Agreements. The relevant plans are referenced in **Appendix 7** and included on CD as **Appendix 8**.

Q2 *With respects to the temporary works on Parkfield Way, the Developer will be required to reinstate the existing junction layout once construction works are complete. A mechanism would need to be installed within the highway works agreement in order to the Developer to reinstate the junction upon completion of the works. It is advised that this should be built into the CMP too.*

A2 As above, these plans are being re-submitted as part of the package to WSCC for technical approval (W/C 24.01.11) and commencement of the Legal Agreement process. The letter to WSCC will seek confirmation of which Agreement WSCC wish to use ( S38 / S111 / S278 ), and will state that an appropriate Clause needs to be included to deal with the reinstatement to the existing layout when construction works on the site are complete. Paragraph 5.22 to has been updated to outline that the reinstatement works will be undertaken following the completion of building work on the site. The relevant plans are referenced in **Appendix 7** and included on CD as **Appendix 8**.

Q3 *Works have also been promoted to Bolnore Road where construction vehicles are to enter by way of a new vehicular access. The Bolnore Road improvement works previously promoted include carriageway surfacing, provision of a passing place, additional highway surface water drainage, and provision of a footway along the southern side. In light of the significant increase in HGV movements on Bolnore Road, it would be envisaged there being a need to implement these works, particularly the footway.*

A3 The Works to Bolnore Road are still proposed. These, along with the other off-site Highway Works listed above are being re-submitted to WSCC (W/C 24.01.11) for technical approval and commencement of the Legal Agreements. The relevant plans are referenced in **Appendix 7** and included on CD as **Appendix 8**. Hard copies of the plans listed also accompany this submission.

- Q4** A temporary scheme of waiting restrictions has been previously implemented along Bolnore Road due to there being anticipation of the construction works commencing. The order for these temporary restrictions was implemented in September 2008 and lasts for a five year period. Upon the expiry of the temporary order, it would be procedurally incorrect to apply a further temporary order. It is therefore advised that if the waiting restrictions are deemed likely to be required beyond the five year period that a permanent TRO is implemented that should then be revoked as required upon cessation of the construction works. It would be beneficial for this to be considered against the duration of construction works and the one way construction routing, and if necessary a permanent order implemented with all associated costs met by the Developer and for these process to be commenced as soon as possible.
- A5** The developer is aware that the temporary TRO is due to expire in 2013. Therefore a permanent TRO will be applied for in due course, with all reasonable costs associated with this work to borne by the developer.
- Q5** There is a separate condition (no. 42) relating to the treatment of the pinch point between phase 4a and phase 4b which require details to be provided. Whilst this condition is not sought to be discharged, construction traffic will need to access through this pinch point. It is appreciated that this is a separate issue but notwithstanding any comments relating to the construction traffic routing in principle, the matter of construction traffic through the pinch point will need to be considered as part of the CMP.
- A5** An additional section in relation to the Pinch Point has been added to this CMP at paragraph 5.41.

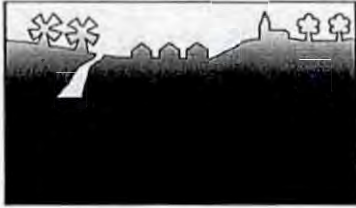
## 7.0 Conclusion

### 7.1 This CMP has demonstrated that:

- Extensive preventative measures will be put in place to control construction access and noise as per the conditions attached to application **09/02101/COND** and the key elements within the Statement of Common Ground.
- Impacts associated with construction traffic will be kept to an absolute minimum and due diligence will be used.
- It is anticipated that the Local Planning and Highway Authorities will commit to this CMP by recognising the proactive approach adopted by Crest Nicholson to the necessary issue of construction access and construction traffic.
- It is acknowledged that small variations to the CMP may be necessary as development progresses and the Phasing programme may have to be subject to appropriate alterations due to ongoing developments on the ground. This can be addressed through an "open book approach", for the full benefit of MSDC, WSCC and local residents' groups.
- Crest Nicholson's commitment to the CMP at this stage demonstrates consideration, structure and robustness to the process. The flexibility of the CMP provides the ability to deal proactively with construction traffic through an efficient, well-managed regime to the benefit of all parties involved.
- The CMP can be seen as a means to an end. Construction is a temporary, but an essential activity associated with all developments.



## Appendix 1



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09/02101/COND

TOWN AND COUNTRY PLANNING ACT, 1990

## PERMISSION

Description: : APPLICATION FOR THE VARIATION/REMOVAL OF CONDITIONS 1, 4, 5, 12, 32, 34, 40, 41, 42, 46 ASSOCIATED WITH OUTLINE PLANNING PERMISSION 04/02676/OUT (PHASE 4)

Location: : PHASES 4A BOLNORE VILLAGE DEVELOPMENT, PARKFIELD WAY, HAYWARDS HEATH.

The Council hereby notify you that they **GRANT** planning permission for the above development to be carried out in accordance with the submitted application and plans and subject to compliance with the following conditions:-

### *Time limit / Exercising the permission*

1. Approval of the details of the layout, scale and appearance of the buildings, landscaping and means of access thereto, excluding the points of entry to the site (including the spine road layout, if amended from the approved masterplan) of the sub-phase areas (hereinafter referred to as 'sub-phase' or 'sub-phases' as the case may be) and as each shown on the approved masterplan drawing (1469.2-A-101-C) (hereinafter called 'the reserved matters') shall be obtained from the local planning authority in writing prior to the commencement of development hereby permitted on any part of Phases 4 and 5 comprising a number of dwellings and associated development such as roads and other infrastructure to be completed and proposed to be released for occupation as a discrete parcel. For the purposes of this condition the sub-phases areas are Nos. 4A, 4B1-4B4, 4C, 5.

Reason: To enable the Local Planning Authority to control the development in detail and to comply with section 92 of the Town and Country Planning Act 1990.

2. Application for the approval of the reserved matters shall be made to the local planning authority before the expiration of five years from the date of this permission.

Reason: To enable the Local Planning Authority to control the development in detail and to comply with section 92 of the Town and Country Planning Act 1990.

3. The development hereby permitted shall be begun not later than the expiration of two years from the date of approval of the last of the reserved matters to be approved.

Reason: To enable the Local Planning Authority to control the development in detail and to comply with section 92 of the Town and Country Planning Act 1990.

4. The permission hereby granted relates to the following schedule of plans/drawings:

1:2500 Site Location Plan (1079/015/B)  
Masterplan 1469.2-A-1010-C  
Environmental Statement, dated June 2009

Reason: For the avoidance of doubt and to comply with Policy HH2 of the Mid Sussex Local Plan.

5. The development hereby permitted shall be constructed and laid out in broad accordance with the mix and disposition of uses shown on the Masterplan 1469.2-A-1010-C or as may be varied by condition attached to this permission.

Reason: For the avoidance of doubt and to comply with Policy HH2 of the Mid Sussex Local Plan.

6. The development shall not be carried out otherwise than in accordance with the reserved matters approved pursuant to condition 1 of this permission.

Reason: To ensure a satisfactory form of development and to comply with Policy HH2 of the Mid Sussex Local Plan.

#### **Play Areas**

7. The layout of each sub-phase of the development shall incorporate provision for play space and play equipment in accordance with details to be approved in writing by the local planning authority and the sites provided for such facilities shall not thereafter be used for any other purpose. Within each sub-phase, no more than 50% of the dwellings therein shall be occupied until the provision for that sub-phase has been completed, provided that 50% of any dwellings within 25 metres of the play area have been constructed and are ready for occupation.

Reason: To ensure satisfactory provisional equipment and to ensure that play areas are provided and retained within the development for use by the general public and to accord with Policy R3 of the Mid Sussex Local Plan.

8. Before the release for sale to prospective purchasers of the first dwelling in each sub-phase, signs shall be erected on the site of all proposed play areas indicating the intention to provide play areas on those sites. The details of such signage shall be submitted to and approved in writing by the local planning authority and maintained in accordance with such approved details until the play areas are provided.

Reason: To ensure that prospective purchasers are aware of proposals for the provision of play areas and to accord with Policy R3 of the Mid Sussex Local Plan.

9. The areas of land identified as open space, woodland and buffer zones on the approved Masterplan and as may be varied by reserved matters approvals shall remain undeveloped and shall be laid out as amenity land in association with the remainder of the land the subject of this permission, such amenity land to be laid out



and landscaped in accordance with conditions Nos. 1, and 21. The land shall not be included in any residential curtilage. These areas shall be maintained in accordance with a Landscape Management Plan and an Ecological Management Plan, which shall have been submitted to and approved in writing by the local planning authority prior to commencement of development of any sub-phase.

Reason: To protect the ecological value of the site and to accord with Policy C5 of the Mid Sussex Local Plan.

#### ***Fencing/screen walling***

10. No development shall take place in each sub-phase until details of proposed screen walls/fences and/or hedges have been submitted to and approved in writing by the local planning authority and no dwellings shall be occupied until such screen walls/fences or hedges associated with them have been erected or planted.

Reason: In order to protect the appearance of the area and to accord with and Policy H3 of the Mid Sussex Local Plan.

#### ***Highways and parking***

11. No dwelling within any sub-phase shall be occupied until details of the space to be laid out for parking and stationing of vehicles clear of the public carriageway, within that sub-phase, have been approved in writing by the local planning authority. No dwelling within each sub-phase shall be occupied until the parking space relating to that dwelling has been surfaced, drained and completed. The space so provided shall not thereafter be used other than for the parking of vehicles in connection with the development hereby permitted.

Reason: To ensure that adequate and satisfactory provision is made for the accommodation of vehicles clear of the highways and to accord with Policy T5 of the Mid Sussex Local Plan.

12. Detailed plans referred to in condition 1 of this permission shall provide for car parking at an average maximum of 2.5 off-street car parking spaces per dwelling.

Reason: To ensure that adequate and satisfactory provision is made for the accommodation of vehicles clear of the highways and to accord with Policy T5 of the Mid Sussex Local Plan.

13. No dwelling construction shall commence until details of the bus-only link and emergency vehicle access control mechanisms have been submitted to and approved in writing by the local planning authority. No dwelling shall be occupied until such mechanisms are in place or alternative temporary controls are in place in accordance with details to be submitted to and approved in writing by the local planning authority.

Reason: To ensure that this element of the scheme is satisfactorily controlled and to comply with Policy HH2 of the Mid Sussex Local Plan.

14. Each individual dwelling shall not be occupied until provision has been made within the site, in accordance with details to be submitted to and approved in writing by the local planning authority, for the parking of bicycles for each such dwelling clear of the public highway and such space shall not thereafter be used other than for the purposes for which it is provided.

Reason: To ensure that adequate provision is made and to comply with Policy T6 of the Mid Sussex Local Plan.

15. No development within any sub-phase shall take place unless and until details of the layout and specifications of, and construction programmes for, the roads (including visibility splays), footpaths, cycleways and casual parking areas in that sub-phase have been submitted to and approved in writing by the local planning authority. No dwelling shall be occupied until it is provided with access constructed in accordance with such approved details to the established highway network.

Reason: In the interests of road safety and to accord with Policy T4 of the Mid Sussex Local Plan.

16. Except as approved as part of this permission (including development traffic and access to the sub-phase identified as 4C on the approved Masterplan) means of vehicular access to the development site shall not be otherwise than from the new estate roads leading to the Haywards Heath Relief Road. No means of vehicular access shall be created to Bolnore Road, Duncton Close, Climping Close or Wealden Way (including via Phase 5) otherwise than pursuant to condition 13.

Reason: In the interest of road safety and to comply with Policy HH2 of the Mid Sussex Local Plan.

17. This permission does not include the levels, gradients, means of construction or visibility splays of the bridleway crossings to Parkfield Way and Renfields unless previously approved and implemented pursuant to application ref. HH/06/0246/FUL. No work on these access roads shall be commenced until details of these matters have been submitted to and approved in writing by the local planning authority and the development shall not be carried out otherwise than in accordance with such details.

Reason: In the interest of road safety and to comply with Policy T4 of the Mid Sussex Local Plan.

18. No dwelling shall be occupied until details of the off-site highway works shown on the drawings pursuant to application ref. HH/04/02965/FUL have been submitted to and approved in writing by the local planning authority and thereafter fully implemented.

Reason: In the interest of road safety and to comply with Policy T4 of the Mid Sussex Local Plan.

### ***Landscaping***

19. No development of any sub-phase shall take place unless and until there has been submitted to and approved in writing by the local planning authority a scheme of hard and soft landscaping, which shall include indications of all existing trees and hedgerows on the land, and details of any to be retained, together with measures for their protection in the course of development.

Reason: In the interests of visual amenity and of the environment of the development and to accord with Policy B1 of the Mid Sussex Local Plan.

20. Hard and soft landscape works shall be carried out in accordance with the approved details. The works within each sub-phase shall be carried out prior to the occupation



of any part of that sub-phase or in accordance with a programme agreed in writing by the local planning authority. Any trees or plants which within a period of five years from the completion of the development die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of similar size and species unless the local planning authority gives written consent to any variation.

Reason: In the interests of visual amenity and of the environment of the development and to accord with Policy B1 of the Mid Sussex Local Plan.

21. A Landscape Management Plan, including ecological supervision and long-term design objectives, management responsibilities, arboricultural supervision and maintenance schedules for all landscape and open areas, other than small, privately-owned domestic gardens, shall be submitted to and approved in writing by the local planning authority prior to the occupation of any sub-phase of the development. The Landscape Management Plan shall be carried out as approved.

Reason: In order to protect the ecological value of the site and to comply with Policy C6 of the Mid Sussex Local Plan.

#### ***Hedge/tree protection***

22. The burning of materials obtained by site clearance or from any other source shall not take place within the areas identified as buffer zones or within 10 metres of the furthest extent of the canopy of any tree or group of trees to be retained on the site or on land adjoining.

Reason: To protect trees and vegetation from fire damage and to accord with Policy B1 of the Mid Sussex Local Plan.

23. No trenches or pipe runs for services and drains required to service the development hereby approved shall be cut or laid otherwise than in accordance with BS 5837:2005, except with the prior written approval of the local planning authority.

Reason: To protect tree roots from damage likely to lead to the loss of a tree and to accord with Policy B1 of the Mid Sussex Local Plan.

24. No development shall take place on the site unless and until all existing trees, shrubs, hedges and buffer zones to be retained within and abutting each phase have been protected by a fence to be approved in writing by the local planning authority; this shall be installed around each tree or group of vegetation in accordance with BS5837:2005, or in accordance with the scheme for the protection of buffers adjacent to ancient woodland, as appropriate, or such distance as may otherwise be agreed in writing by the local planning authority. Within the areas so fenced off, and except as may be required by the approved Landscape and/or Ecological Management Plan, the existing ground levels shall be neither raised nor lowered and no materials, temporary buildings, plant, machinery or surplus soil shall be placed or stored thereon without the prior written approval of the local planning authority. If any trenches or services are required in the fenced-off areas they shall be excavated and backfilled by hand.

Reason: To ensure the retention and maintenance of trees and vegetation which is an important feature of the area and to accord with Policy B1 of the Mid Sussex Local Plan.

25. No development shall take place on any part of Phase 4 until there has been submitted to and approved in writing by the Local Planning Authority a scheme to



protect buffer zones providing at least 15 metre separation between development and the edge of woodland within Reading wood, Four Acre Wood, Bridleway Shaw and Quarry Hollow. Development shall not be implemented otherwise than in accordance with the approved scheme.

Reason: To protect the ecological value of the site and to comply with Policy C6 of the Mid Sussex Local Plan.

#### **Materials**

26. No development shall be carried out in each sub-phase unless and until a schedule of materials and finishes to be used for external walls and roofs of the proposed dwellings within each sub-phase has been submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

Reason: To enable the Local Planning Authority to control the development in detail in the interests of amenity by endeavouring to achieve a building of visual quality and to accord with Policy B1 of the Mid Sussex Local Plan.

#### **Regulation following development**

27. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 (or any order revoking and re-enacting that Order with or without modification) no gate, fence, wall or other means of enclosure exceeding 1 metre in height above ground level shall be erected, or constructed in front of the forwardmost part of any building which fronts an open space, highway, footpath or cycleway unless planning permission is specifically granted by the local planning authority on an application in that regard.

Reason: In order to safeguard the character and visual amenities of the locality and to accord with Policy B1 of the Mid Sussex Local Plan.

#### **Controls during construction**

28. No work for the implementation of the development hereby permitted shall be undertaken on the site on Public Holidays. Work shall not be undertaken at any other time except between the hours of 08.00 and 18.00 on Mondays to Fridays and between 08.00 and 13.00 on Saturdays, unless otherwise agreed in writing by the local planning authority.

Reason: To safeguard the amenities of nearby residents and to accord with Policy B3 of the Mid Sussex Local Plan.

29. Except as already provided for in condition 29 the implementation of the development hereby permitted shall be undertaken in accordance with the following:

Access to the site for HGVs over 7.5 tonnes will be limited to between 09.15 and 15.30, Mondays to Fridays;

Access to the site for heavy plant over 7.5 tonnes will be limited to between 11.00 and 15.00, Mondays to Fridays; and

Access to the site for articulated vehicles (other than heavy plant) will be limited to between 09.15 and 15.30, Mondays to Fridays.

Reason: To safeguard the amenities of nearby residents and to accord with Policy B3 of the Mid Sussex Local Plan.

30. No work shall be carried out on the site unless and until an effective wheel-cleaning facility has been installed in accordance with details to be submitted to and approved in writing by the local planning authority and such facility shall be retained in working order and, unless otherwise agreed in writing by the local planning authority, operated throughout the period of work on the site.

Reason: To ensure that vehicles do not leave the site carrying earth and mud on their wheels in a quantity which causes a nuisance or hazard on the road system in the locality and to accord with Policy T4 of the Mid Sussex Local Plan.

31. No work shall be carried out on the site unless there is available within the site, in accordance with details approved in writing by the local planning authority, provision for the temporary parking of vehicles and the loading and unloading of vehicles associated with the building or other operations on the site throughout the period of work required to implement the development hereby permitted.

Reason: In the interests of road safety and to accord with Policy T4 of the Mid Sussex Local Plan.

32. No development shall take place until a Construction Management Plan has been submitted to and approved in writing by the local planning authority. The Construction Management Plan shall incorporate matters set out within the Statement of Common Ground relating to a Construction Management Plan, dated 27 April 2006. Development shall be carried out in accordance with the approved plan.

Reason: To safeguard the amenities of nearby residents and to accord with Policy B3 of the Mid Sussex Local Plan.

#### **Construction Access**

33. Except as set out in Condition 34 vehicles used in the construction of the development, including vehicles of persons employed, shall not enter the site otherwise than from Bolnore Road and shall not exit otherwise than through Parkfield Way.

Reason: To safeguard the amenities of nearby residents and to accord with Policy B3 of the Mid Sussex Local Plan.

34. Two-way access shall be permitted along Parkfield Way to Phase 4A but not to Phase 4B and along Bolnore Road to Phase 4B but not to 4A solely for the purpose of constructing the spine road between Bolnore Road and Parkfield Way, up to base level, and for no other purpose. The timing of the works and two-way working shall be in accordance with a programme to be submitted to and approved in writing by the local planning authority.

Reason: To safeguard the amenities of nearby residents and to accord with Policy B3 of the Mid Sussex Local Plan.

#### **Additional Details**

35. The development hereby permitted shall not commence until details of the proposed surface and foul drainage and means of disposal, including, where relevant, off-site drainage improvements, have been submitted to and approved in writing by the local planning authority and no building shall be occupied until all drainage works have been carried out in accordance with such details.



Reason: To ensure that the proposed development is satisfactorily drained and to accord with Policy CS13 of the Mid Sussex Local Plan.

36. No development within any sub-phase shall take place until details of existing and proposed site levels have been submitted to and approved in writing by the local planning authority. Development shall not be implemented otherwise than in accordance with such details.

Reason: For the avoidance of doubt and to ensure that the development does not prejudice the amenities of adjacent residents or the appearance of the locality and to accord with Policy B1 of the Mid Sussex Local Plan.

### **Archaeology**

37. During permitted hours of site working, with the agreement of the site manager, the developer shall afford access to a qualified field archaeologist, who has been agreed in writing by the developer and by the local planning authority, and shall allow the archaeologist to observe the excavations and record archaeological evidence that may be uncovered as a result of the development.

Reason: The site is potentially of archaeological interest and to accord with B18 of the Mid Sussex Local Plan.

38. No development shall take place within any sub-phase until there has been secured by the applicant/developer the implementation of a programme of archaeological evaluation/recording within that sub-phase in accordance with a written scheme of investigation, which has been submitted to and approved in writing by the local planning authority.

Reason: The site is of archaeological significance and it is important that it is recorded by excavation before it is destroyed by development and to accord with Policy B18 of the Mid Sussex Local Plan.

### **Bridleway**

39. No dwelling shall be occupied until the works to upgrade the bridleway, approved under planning permission Ref. HH/06/0246/FUL, (or alternative such planning permission as may be approved), have been completed and these shall include the cross-over works in connection with the upgrade.

Reason: To ensure that suitable alternative routes are available to residents and to comply with Policy T4 of the Mid Sussex Local Plan.

### **Housing**

40. Each sub-phase of the residential development hereby permitted shall have an average density of not less than 30 dwellings per hectare.

Reason: To ensure an efficient use of land and to accord with Policy H3 of the Mid Sussex Local Plan.

### **Ecology**

41. Prior to the commencement of construction within each sub-phase an Ecological Management Plan shall be submitted to and approved in writing by the local planning authority and shall include detailed specifications and management regimes as appropriate for the following:



(a) Woodland buffering, including the orientation of dwellings proposed to adjoin the woodland buffer zones

(b) Acid grassland

(c) Nature conservation and ecological mitigation including details of habitat and soils translocation (to include details of monitoring of outcomes over time) and enhancement where necessary

(d) Protected species and their habitats (including proposals for licensing applications where necessary)

(e) Appointment of an ecological clerk of works to monitor construction activities that may impact on the above.

Development shall be carried out in accordance with the approved plan.

Reason: To protect the ecological value of the site and to accord with Policy C5 of the Mid Sussex Local Plan.

#### ***Pinch-point crossing***

42. No development shall take place until detailed proposals for the design, construction and management of the Pinch-point crossing between Phases 4A and 4B have been submitted to and approved in writing by the local planning authority. These proposals shall incorporate the protection of adjacent trees by use of road construction design, methods and materials (including a porous surface) that will facilitate the passage of wildlife beneath the road surface; location of sub-surface services to avoid root damage; and proposals to secure continuous ecological linkages from Reading Wood to North Meadow uninterrupted by side fencing or other obstructions. Development shall be carried out in accordance with the approved details.

Reason: To ensure that this aspect of the development is acceptable.

#### ***Limits on Number of Dwellings***

43. Excluding development on Phase 4C, no more than 310 dwellings shall be occupied within Phase 4 until Stages 5 and 6 of the Haywards Heath Relief Road have been constructed and are open to public traffic.

Reason: In the interest of highway safety and to comply with Policy HH2 of the Mid Sussex Local Plan.

44. The number of dwellings constructed within the housing development sub-phases identified on the Masterplan 1469.2-A-1010-C shall not exceed 543.

#### **INFORMATIVES**

1. You are advised that this planning permission requires compliance with a planning condition(s) **before development commences**. You are therefore advised to contact the case officer as soon as possible. If you carry out works prior to a pre-development condition being discharged then a lawful start will not have been made and you will be liable to enforcement action.

**Reason for Decision**

1. The application has been rigorously assessed by the District Valuer (DV), instructed by the Council to independently assess the applicants' claims. Due to the special circumstances surrounding this case – the current economic climate, the high infrastructure cost, district wide significance, officers have negotiated a position based upon the advice received from the DV that ensures a reasonable and appropriate package can be secured to deliver the HHRR within a specific time scale, provide a level of affordable housing, bring about some community gains in respect of a community site and Bridleway enhancements, as well as other contributions.

Carefully consideration has been given to the proposed variations in conditions being sought by the applicant and where some flexibility will be of benefit, i.e. parking standards and density, variations can be supported. Where there are implications on residential amenity, i.e. construction traffic, than variations have not been supported and the conditions have been reapplied.

It is felt that the overall package is fair and reasonable and will meet the aspirations of the Council and applicants while ensuring conformity to policies HH2, H3, H4, B3, T4 and T5 of the Mid Sussex Local Plan.

*This information is only intended as a summary of the reasons for grant of planning permission. For further detail on the decision please come and view the planning file at the council offices during normal working hours (9 am – 5 pm Monday – Thursday and 9 am – 4 pm Friday).*

  
HEAD OF ECONOMIC PROMOTION AND PLANNING

29<sup>TH</sup> OCT 2010

NB: IT IS IMPORTANT THAT YOU SHOULD READ THE NOTES ACCOMPANYING THIS FORM

PEFULZ





P1	PRELIMINARY ISSUE	EX SEP 08
Issue	Description	Date
Status <div style="text-align: center;"> <b>PRELIMINARY</b>  <b>NOT TO BE USED FOR CONSTRUCTION</b> </div>		
Scale	Current Issue Signatures	
	NTS	Author P. Thewissen
Original Scale	A1	Checker A. (unintelligible)
Height		Approver B. (unintelligible)
Bottom		Drawn B. (unintelligible)
Grid		© Copyright reserved
Filename: FIG 2 CONSTRUCTION TRAFFIC TWO WAY DWG		
Client:		
 <b>Crest</b> <b>NICHOLSON</b> Crest Nicholson (South) Ltd Crown House, Russell Road, Elmbridge, Surrey, KT11 3JN Tel: 0181 82 33444 Fax: 0181 823 3344		
 Bettridge Turner and Partners (in Director of Highway Consulting Ltd) Limited  <div style="float: right;">           BTRP - HYDER            85 King Edward            Mansions            Kent            ME1 1BE            Tel: 01622 476 177            Fax: 01622 741 919         </div>		
Project:		
<b>BOLNORE VILLAGE</b> <b>PHASE 4/5</b>		
Title:		
<b>CONSTRUCTION TRAFFIC ROUTE</b> <b>FOR TWO WAY WORKING</b> <b>(REQUIRED UNTIL COMPLETION</b> <b>OF SPINE ROAD)</b>		
Drawing No.	Project No.	Issue
FIG 2	GD08041	01



## Appendix 2

## Appendix 3



PI	PRELIMINARY ISSUE		39 SEP 84
Issue	Description:		Date
Status	<p align="center"><b>PRELIMINARY</b> NOT TO BE USED FOR CONSTRUCTION</p>		
Scopes	Current Issue Signatures		
	NTS	Author P. Thurnau	
Original Scale	A1	Checker J. Shuman	
Height Datum		Approvers J. Seach	
Grid		© Copyright reserved	
Filemark	FIG 1 CONSTRUCTION TRAFFIC ONE WAY DWS		
Client	 <b>Crest</b> <b>NICHOLSON</b> Crest Nicholson / Shumati Ltd One Village Park Road, Sharnbrook, Wetherby, Wetherby, Wetherby, Wetherby Wetherby, Wetherby, Wetherby, Wetherby Wetherby, Wetherby, Wetherby, Wetherby		
	 <b>Bethune Turner and Partners</b> (a Division of Foster Consulting (UK) Limited)		
	 <b>Hycon Consulting</b> BTP 40/200 40 King Street Manchester M2 7LH Tel: +44 (0)161 274 177 Fax: +44 (0)161 274 178		
Project	<p align="center"><b>BOLNORE VILLAGE</b> <b>PHASE 4/5</b></p>		
Title	<p align="center"><b>CONSTRUCTION TRAFFIC ROUTE</b> <b>FOR SINGLE WAY WORKING</b> <b>ENTRY AT BOLNORE ROAD</b> <b>EXIT AT PARKFIELD WAY</b></p>		
Drawing No.	FIG 1	Project No.	GD08041 - 01





0 20 40m  
1:1

P1	PRELIMINARY ISSUE	25 APR 08
Issue	Description	Date

Client  
**CREST NICHOLSON**

Status	<b>PRELIMINARY NOT TO BE USED FOR CONSTRUCTION</b>	
Scales	1500	Current Issue Signatures
Original Size	A3	Author J. RALPH
Height		Checker B. GOOCH
Datum		Approver B. GOOCH
Grid		Copyright reserved
Filename	SK2.DWG	

Project  
**HAYWARDS HEATH  
OFF SITE WORKS**

Title  
**PARKFIELD WAY  
ARTICULATED TRACK PLOT**

**BT&P** Beltridge Turner and Partners  
UK Division of Hyder Consulting (UK) Limited

**Hyder** Consulting

BTW JAY008  
89 King Street  
Reading  
RG1 1AG  
Tel: +44 (0)1832 314111  
Fax: +44 (0)1832 314132

Drawing No: **SK2** - Project No: **26138** - Issue: **P1**

## Appendix 4



Key



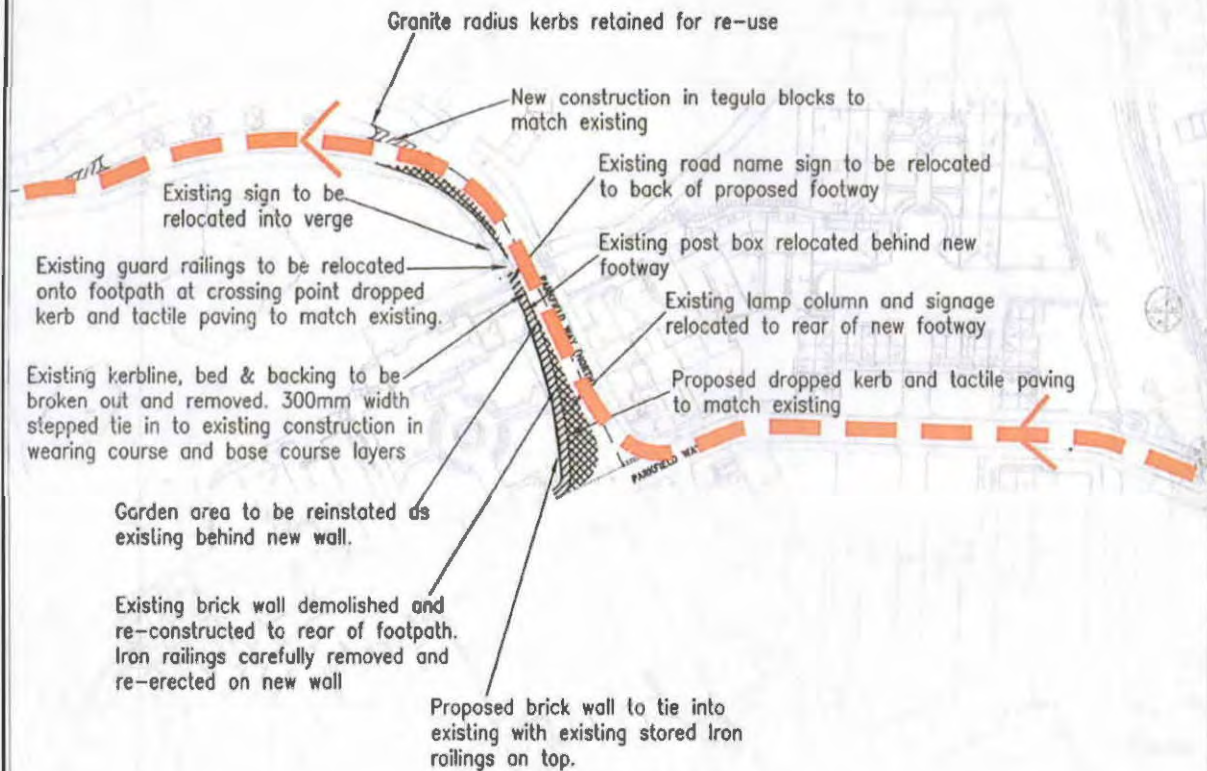
Proposed Carriageway construction



Proposed block paved Footway construction to match existing

Note

Granite kerbs to be retained for re-use








## Appendix 6

**Bolnore Village Phases 4 and 5  
Construction Management Plan**

**Photographs of Vehicles used in the Construction Process**

360° Turner	
Moxy Dump Trucks	
Rigid wheelbase HGV for hardcore/tarmac	

8-wheel trucks for muck removal



Concrete mixers



Hi-ab delivery vehicles for other miscellaneous materials







Low loader for the delivery of a piling rig





Vehicles for scaffolding





JCB	
Forklift	
Rigid lorries supplying bricks and other building materials	
Articulated lorries supplying joists and prepared timber products	



<p>Skip lorries</p>	
<p>Tarmac rollers</p>	



## Appendix 7 – Plans provided in relation to WSCC Comments

26138/101	A1	Proposed Parking Restrictions Plan	B
26138/102	A1	Bolnore Road G.A. & Drainage Plan	A
26138/103	A1	A272 / B2184 Roundabout G.A. & Drainage Plan	B
26138/104	A1	Tylers Green Roundabout G.A. & Drainage Plan	C
26138/105	A0	Parkfield Way Roundabout Drainage & Markings plan	C
26138/302	A1	Bolnore Road Setting out & Contour Plan	.
26138/303	A1	A272 / B2184 Roundabout Setting out & Contour Plan	A
26138/304	A1	Tylers Green Roundabout Setting out & Contour Plan	.
26138/305	A0	Parkfield Way Roundabout Setting out & Contour Plan	B
26138/402	A1	Bolnore Road Construction Areas	.
26138/403	A1	A272 / B2184 Roundabout Construction Areas	A
26138/404	A1	Tylers Green Roundabout Construction Areas	A
26138/405	A0	Parkfield Way Roundabout Construction Areas	D
26138/602	A1	Bolnore Road Vertical Alignment	.
26138/603	A1	A272 / B2184 Roundabout Vertical Alignment	A
26138/604	A0	Tylers Green Roundabout Vertical Alignment	.
26138/605	A0	Parkfield Way Roundabout Vertical Alignment	A
26138/801	A1	Construction and Drainage Details	.
26138/802	A1	Construction and Drainage Details	.
S278 - 02	A1	Bolnore Road Section 278 Agreement Plan	.
S278 - 03	A1	A272 / B2184 Roundabout Section 278 Agreement Plan	.
S278 - 04	A2	Tylers Green Roundabout Section 278 Agreement Plan	A
S278 - 05	A1	Parkfield Way Roundabout Section 38/278 Agreement Plan	B